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CORPORATION



A Halliburton Company

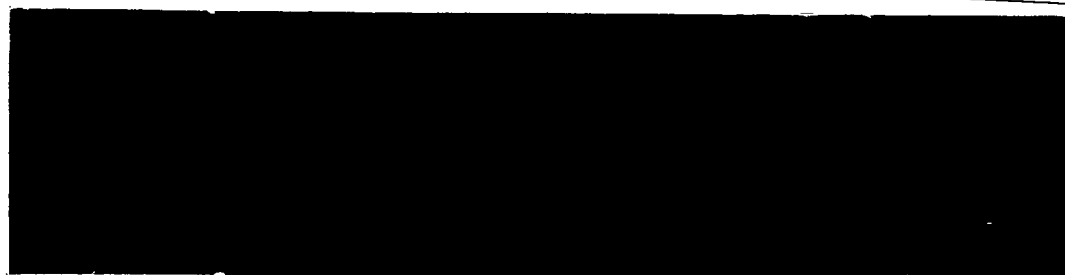
ORIGINAL  
(Red)

806-07-12



SEMS DocID

2334754



PROJECT FOR  
PERFORMANCE OF  
REMEDIAL RESPONSE ACTIVITIES AT  
UNCONTROLLED HAZARDOUS  
SUBSTANCE FACILITIES—ZONE 1

NUS CORPORATION  
SUPERFUND DIVISION

ORIGINAL  
(Red)

R-585-8-6-2  
NON-SAMPLING SITE RECONNAISSANCE SUMMARY REPORT  
FORD MOTOR COMPANY  
PREPARED UNDER

TDD NO. F3-8606-07  
EPA NO. PA-521  
CONTRACT NO. 68-01-6699

FOR THE  
HAZARDOUS SITE CONTROL DIVISION  
U.S. ENVIRONMENTAL PROTECTION AGENCY

AUGUST 22, 1986

NUS CORPORATION  
SUPERFUND DIVISION



992 OLD EAGLE SCHOOL ROAD, SUITE 916  
WAYNE, PENNSYLVANIA 19087  
215-687-9510

our **25**<sup>th</sup> year

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(Red)

August 22, 1986  
R-585-8-6-2  
68-01-6699

Mr. Harold Byer  
U.S. Environmental Protection Agency  
841 Chestnut Building  
Ninth and Chestnut Streets  
Philadelphia, PA 19107

Subject: Non-sampling Site Reconnaissance Summary Report  
TDD No. F3-8606-07  
Ford Motor Company  
Chester, Pennsylvania

Dear Mr. Byer:

NUS FIT III was tasked to conduct a Non-sampling Site Reconnaissance for the subject site. Based on our review of available data and the site visit, we have concluded that EPA should consider the following:

- o No further action should be taken at this site by EPA at this time.

#### **Background Information**

The site consists of approximately 56 acres in Chester, Delaware County, Pennsylvania, which Ford operated from 1927 until 1961. The site included an 11-acre "burning pit," which was filled in with construction debris and leveled prior to 1951. The pit lies in an area of low ground behind a wooden bulkhead adjacent to the Delaware River. The wastes that were disposed of at the pit were organics, inorganics, solvents, heavy metals, and paint sludges. In 1961, the site was sold to the Reynolds Metal Company, Electric Wire and Cable Plant. No wastes were disposed on site by Reynolds and the pit remained unused. In 1981, the site was sold to its current owners, the Delaware County Economic Development Center, Incorporated, and was renamed the Riverbridge Industrial Center. The pit area is presently vacant and vegetated.

Land use in the site vicinity is predominantly residential and industrial. The nearest home is 300 feet from the site.

Mr. Harold Byer  
U.S. Environmental Protection Agency  
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Ford Motor Company Non-Sampling Site Reconnaissance Summary Report

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### Contacts

Robert Zang  
Solid Waste Specialist  
PA DER  
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Norristown, PA  
(215) 270-1920

Albert Hilbert  
Manager  
Riverbridge Industrial Center  
Front and Lloyd Streets  
Chester, PA 19013  
(215) 872-4469

### At the Site

Albert Hilbert  
Manager  
Riverbridge Industrial Center  
Front and Lloyd Streets  
Chester, PA 19103  
(215) 872-4469

### Drinking Water Supply

All residents within the three-mile radius of the site are served by public water companies. The Chester Water Authority serves the area west of Ridley Creek, including the site. The Philadelphia Suburban Water Company supplies water east of Ridley Creek. There are no private wells in the subject area.

### Geology Information

Occupying a region within the Coastal Plain Physiographic Province, the Ford Motor Company lies along the flood plain of the Delaware River. As such, the site is immediately underlain by unconsolidated sedimentary material, referred to as the Trenton gravel. These low-lying gravels are described as gray to pale reddish-brown, very gravelly sand, with interbedded sand and clay silt layers. As a result of their mode of deposition, these materials grade rapidly into one another both laterally and vertically.

Found beneath the Trenton gravel and representing the oldest exposed formation in the region is the Wissahickon Formation. The oligoclase-mica schist facies of the Wissahickon is present in the study region. The oligoclase-mica schist, a medium-grained rock, is described as sparkling gray to green in color and finely plicated. The formation is believed to have been derived from the metamorphism of arkosic sediments.



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SOURCE

- (1) U.S.G.S. Folio 62.
- (2) Postel (1941).
- (3) U.S.G.S. Prof. Paper 1067-D.

PLANATION

- Q1  
Trenton Gravel
- Tpb  
Hansauken and  
Edgeton Fms.,  
undif.
- Xw  
Wissahickon Fm.,  
oligoclase-  
mica schist



REFERENCES

- Baskin, F., Clark, W. B., Darton, N. H., and others (1909). *Philadelphia Folio, Pennsylvania-New Jersey-Delaware*. U. S. Geological Survey Geologic Atlas of the U. S., Folio 162, 23 p.
- Owens, J. P., and Minard, J. P. (1979). *Upper Cambrian sediments of the lower Delaware Valley and the northern Delmarva Peninsula, New Jersey, Pennsylvania, Delaware, and Maryland*. U. S. Geological Survey Professional Paper 1067-D, p. D1-D47.
- Postel, A. W. (1941). *Hydrothermal emplacement of granodiorite near Philadelphia*. Academy of Natural Sciences of Philadelphia, Proceedings, v. 92, p. 123-152.

Compiled by T. M. BERG and  
A. A. SOCOLOW, 1978

BRIDGEPORT  
(N.J. - Pa.)

### Groundwater

A system of joints, reported as irregularly spaced and poorly formed, provides the main openings for groundwater storage and transmission in the Wissahickon Formation. These joints tend to decrease in number and size with depth. Other secondary features, as well as the planes of schistosity, also contribute available void space. The Wissahickon is said to be a fairly good source of water, with a reported median well yield of 20 gallons per minute (gpm).

As for the porosity and permeability of the Trenton gravel, both are reported as high. Where favorable sedimentary conditions exist (e.g., a lens of sand) and where sedimentary thickness is great enough, decent well yields may be obtained; yields in excess of 1,000 gpm are reported. These deposits also act as a recharge source for the underlying bedrock.

No specific information is available for groundwater flow at this time. However, given the site location, shallow groundwater flow is expected to be southward towards the Delaware River.

(The geology and groundwater sections are from Alan R. Geyer and J. Peter Wilshusen, "Engineering Characteristics of the Rocks of Pennsylvania," 1982; and George M. Hall, "Groundwater," 1934.)

### Summary of Activities

On July 22, 1986, a non-sampling site reconnaissance of the subject site was conducted by NUS personnel [redacted] and [redacted]. The NUS members were accompanied by Al Hilbert, of the Riverbridge Industrial Center. The team was on site from 10:17 AM to 11:30 AM. The weather at the time of the inspection was partly sunny and hazy, with a temperature of 85°F.

### Observations

- o The site was vegetated but not evenly graded. There were low areas that showed evidence of ponding, but which did not contain standing water.
- o There are approximately 20 to 30 piles of sand from a roofing manufacturing company in the industrial center.

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Ford Motor Company Non-Sampling Site Reconnaissance Summary Report

- o Two areas of asphalt disposal were observed. The asphalt is also from the roofing company. The asphalt has hardened into solid piles.
- o The site is enclosed by a fence on three sides. The fourth side of the landfill is adjacent to the Delaware River. There are two entrances to the site. The southwest entrance gate is locked, but the north entrance is open.
- o No odors, leachate seeps, or stained soils were observed on site.
- o A trench had been dug to stabilize an anchor drag on the Delaware River. No HNU readings were detected in the trench and no stained soils were noted.
- o The bulkhead along the Delaware River consists of concrete blocks piled along the slope.
- o There are no monitoring wells on site.

#### Conclusions

A site inspection was not recommended because the site poses no threat to public health. All residents in the area are served by municipal water and are therefore not subject to potential contamination of their drinking water supplies. Industrial intakes on the Delaware River are used only for processing and are not consumed. There are no known hazardous wastes disposed of on site and no hazardous conditions were observed on site.

#### Proposed Sample Plan

NUS FIT III does not recommend sampling of the subject site.

If you have any further questions, please contact me.

Respectfully submitted,

Reviewed by,

Approved by,

**Not responsive due to revised scope**

Environmental Technician

Assistant Manager

Manager, FIT III

AHF/kf

Attachments

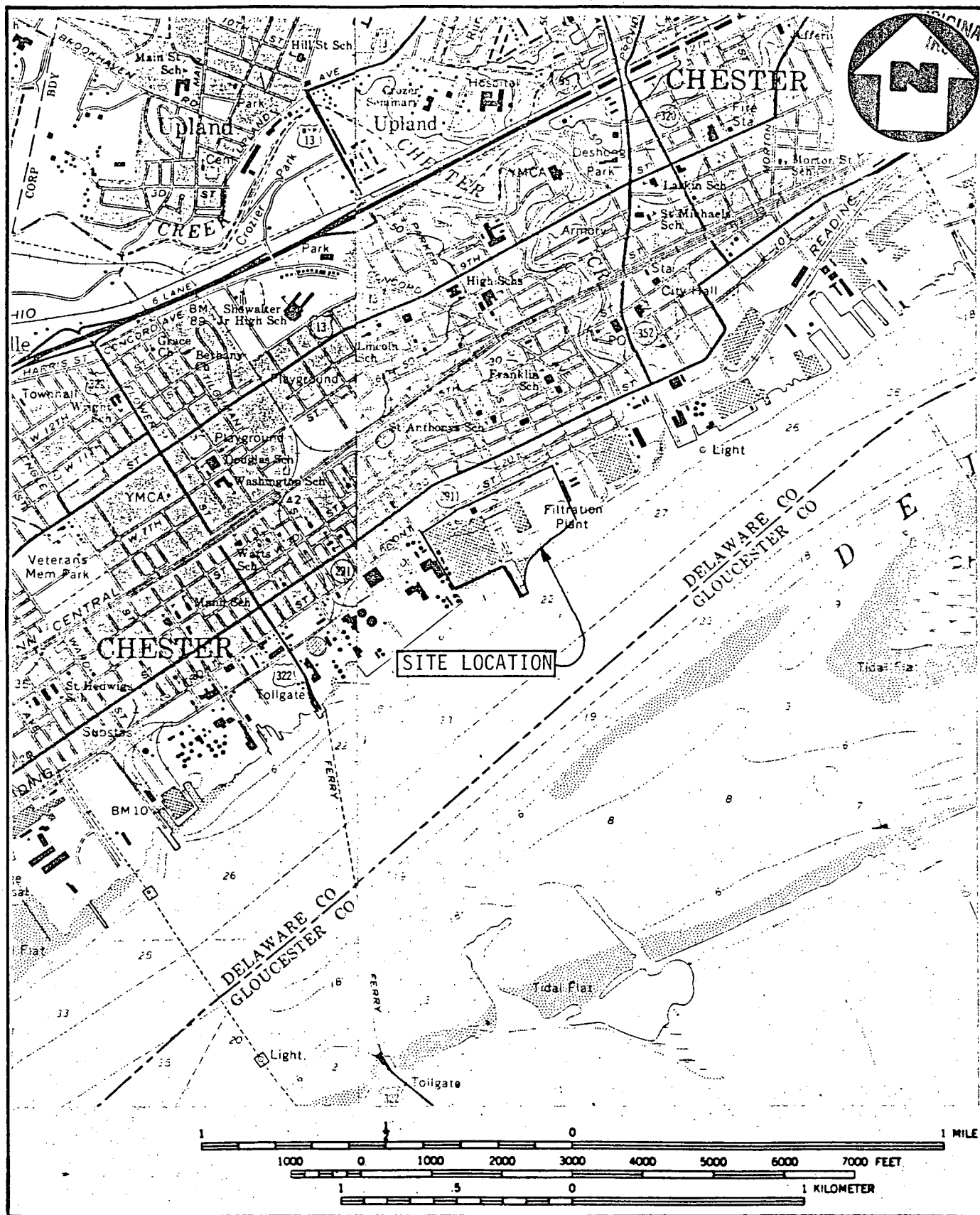
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(Red)

ATTACHMENT 1

1. COST CENTER:	REM/FIT ZONE CONTRACT TECHNICAL DIRECTIVE DOCUMENT (TDD)			2. NO.: F3-8606-07
ACCOUNT NO.:				
3. PRIORITY:  <input type="checkbox"/> HIGH <input type="checkbox"/> MEDIUM <input checked="" type="checkbox"/> LOW	4. ESTIMATE OF TECHNICAL HOURS:  80	5. EPA SITE ID:  PA-521	6. COMPLETION DATE:  8/22/86	7. REFERENCE INFO.:  <input checked="" type="checkbox"/> YES <input type="checkbox"/> NO <input type="checkbox"/> ATTACHED <input checked="" type="checkbox"/> PICK UP
4A. ESTIMATE OF SUBCONTRACT COST:		5A. EPA SITE NAME: <u>Ford Motor Company</u> <u>Chester, PA</u>		
8. GENERAL TASK DESCRIPTION: <u>Conduct a Site Recon and develop a sampling plan of the subject site.</u>				
9. SPECIFIC ELEMENTS:  <u>1.) Review background information.</u> <u>2.) Contact state and local agencies for relevant information.</u> <u>3.) Arrange for site access.</u> <u>4.) Conduct a on and off site inspection to identify potential sample locations</u> <u>(i.e., on site samples, monitoring wells, home wells, etc.).</u> <u>5.) Prepare and submit recon report including proposed sampling plan</u> <u>and rationale, if applicable.</u> <u>6.) All work on this project to be performed according to: WP-PA-1, Rev.1</u>				10. INTERIM DEADLINES:          
11. DESIRED REPORT FORM: FORMAL REPORT <input checked="" type="checkbox"/> LETTER REPORT <input type="checkbox"/> FORMAL BRIEFING <input type="checkbox"/>				
OTHER (SPECIFY): <u>Coordinate with Lorie Acker</u>				
12. COMMENTS: <u>State Code 042 County Code 045</u>				
13. AUTHORIZING RPO:  <u>Harold G. Byer</u> (SIGNATURE)			14. DATE: <u>7/16/86</u>	
15. RECEIVED BY:  <u>Not responsive due to revised scope</u> (CONTRACTOR RPM SIGNATURE)			16. DATE: <u>7/22/86</u>	
OPTIONS <input type="checkbox"/> REJECTED				

ORIGINAL  
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ATTACHMENT 2



SOURCE: (7.5 MINUTE SERIES) USGS BRIDGEPORT & MARCUS HOOK, PA., N.J. QUADS.


# SITE LOCATION MAP

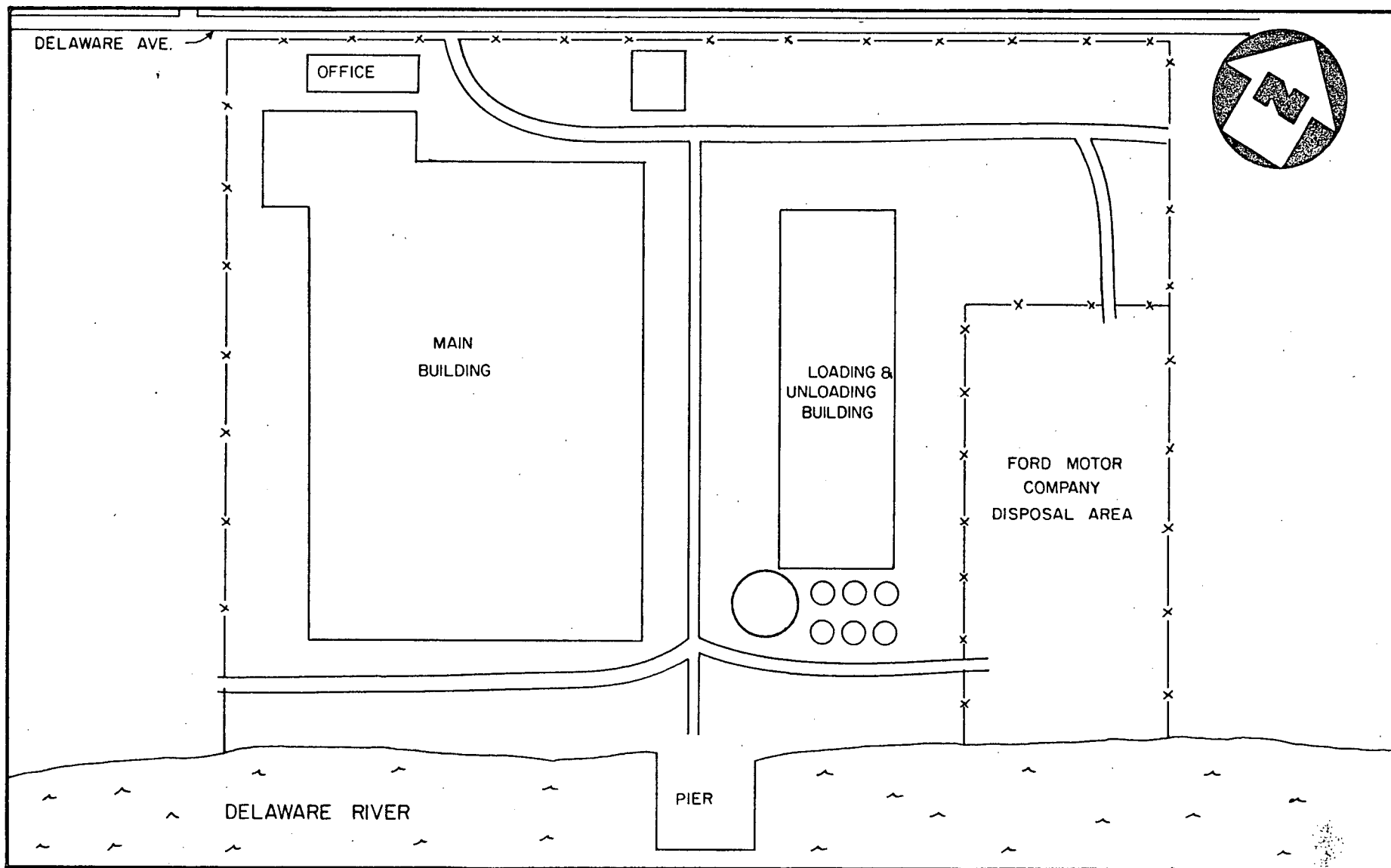
FORD MOTOR CO

SCALE 1:24000

FIGURE 1



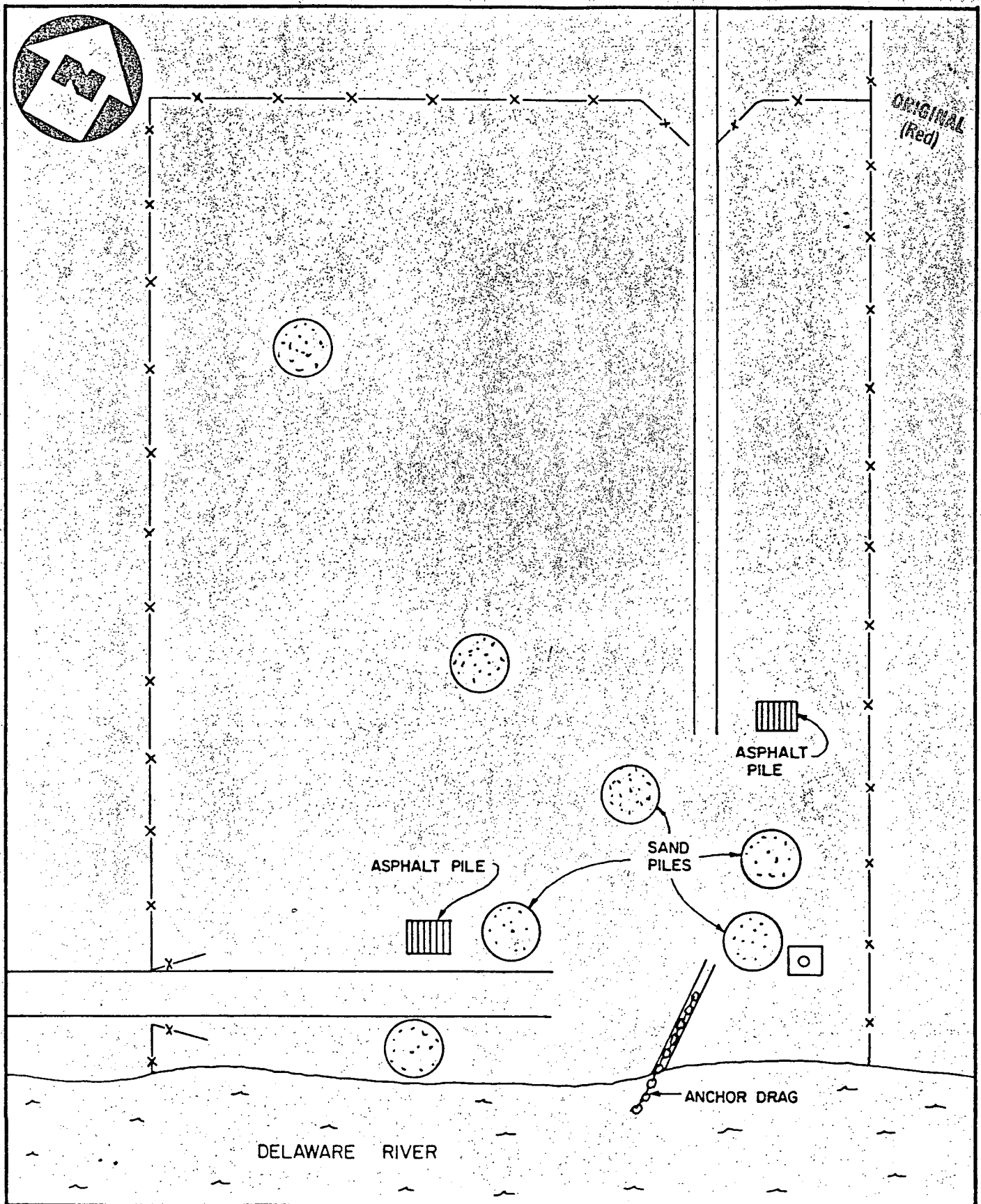
 A Halliburton Company



OVERALL LAYOUT  
FORD MOTOR CO.  
(NO SCALE)

FIGURE - 2

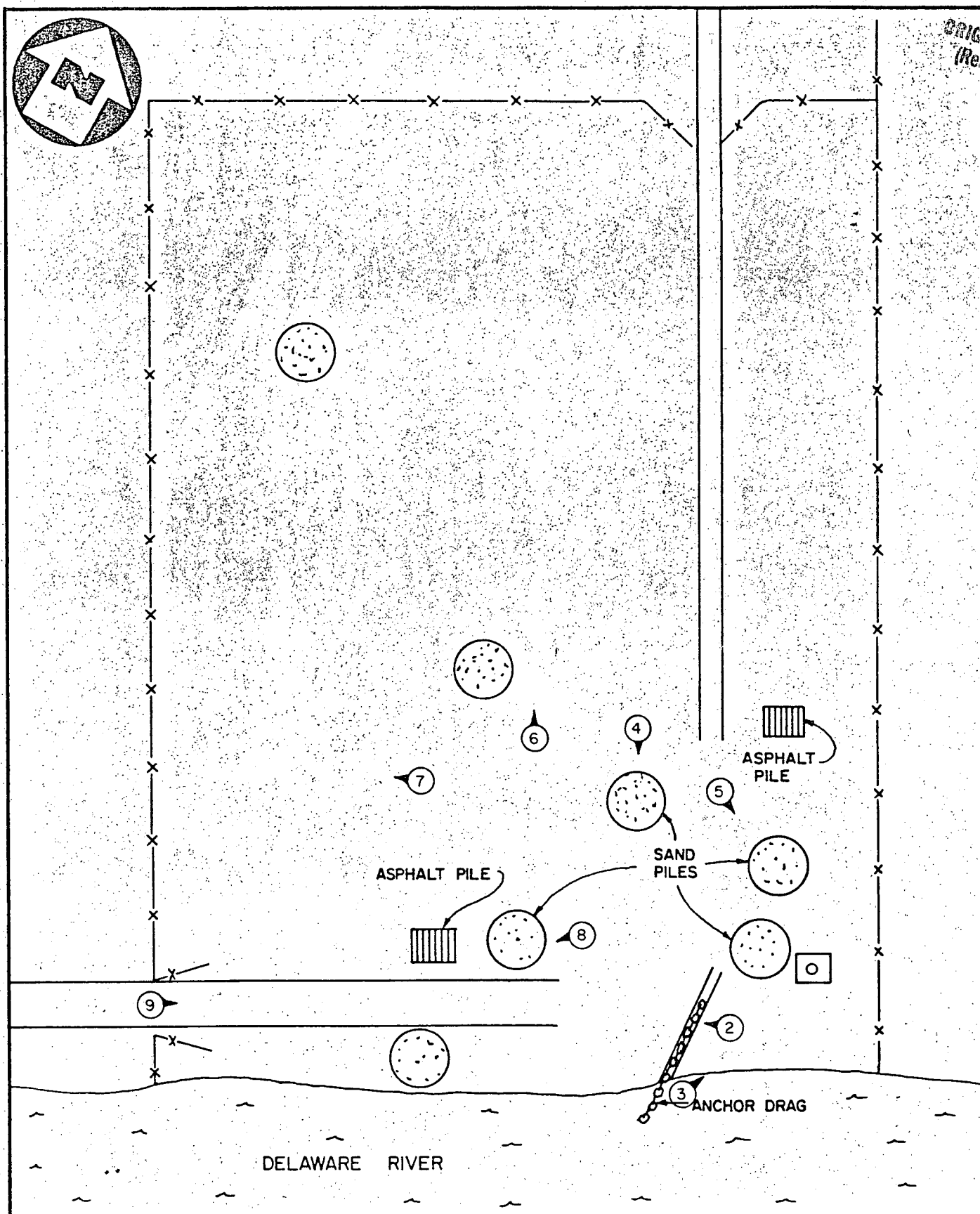




**SITE SKETCH**  
**FORD MOTOR CO.**  
 (NO SCALE)

FIGURE - 3

ORIGINAL  
(Rel)



**PHOTO LOCATION MAP**  
**FORD MOTOR CO.**  
(NO SCALE)

FIGURE - 4

ORIGINAL  
(Red)

ATTACHMENT 3





Photo 1 -  
Anchor trench with asphalt debris. ORIGINAL  
(Red)



Photo 2 -  
Slope from pit to Delaware River.



Ford Motor Company  
F3-8606-07

RIP2  
Photo #1

Anchor trench with asphalt debris.

7/22/86

1050

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ORIGINAL  
(1050)

Photo 1 -

Ford Motor Company  
F3-8606-07

RIP3  
Photo #2

Slope from pit to Delaware River.

7/22/86

1055

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Slope from pit to Delaware River.

Photo 2 -



Photo 3 and 4 -  
Panorama of pit.

ORIGINAL  
(Red)



Ford Motor Company  
F3-8606-07

RIP4  
Photo #3

RIP5  
Photo #4

Panorama of pit.

panorama of pit

7/22/86

1100

1100

Not responsive due to revised scope

Panorama of pit.  
Photo 3 and 4

ORIGINAL  
(100)



Photo 5 -  
North side of fill area.

ORIGINAL  
(Red)



Photo 6 -  
View facing west towards industrial  
center.



Ford Motor Company  
F3-8606-07

RIP6  
Photo #5

North side of fill area

7/22/86

1105

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ORIGINAL  
(Red)

Photo 5 -  
North side of fill area.

FORD MOTOR COMPANY  
F3-8606-07

RIP7  
Photo #6

View facing west towards industrial center

7/22/86

1106

Not responsive due to revised scope

Photo 6 -  
View facing west towards industrial  
center.



— Photo 7 - ORIGINAL  
 — View facing southwest and showing (Red)  
 — open entrance gate. —



— Photo 8 -  
 — View along access road facing northeast. —



Ford Motor Company  
F3-8606-07

RIP8  
Photo #7

View facing southwest and showing  
open entrance gate.

7/22/86

1106

Not responsive due to revised scope

Photo 7 -  
View facing southwest and showing  
open entrance gate.

Ford Motor Company  
F3-8606-07

RIP9  
Photo #8

View along access road facing northeast.

7/22/86

1109

Not responsive due to revised scope

Photo 8 -  
View along access road facing northeast.